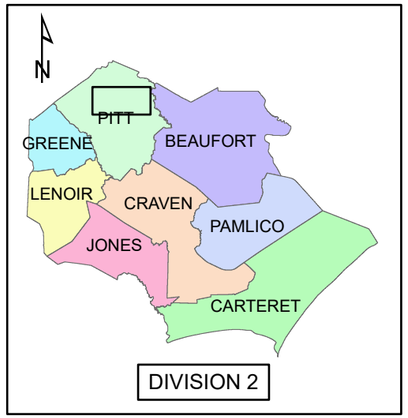


PITT COUNTY

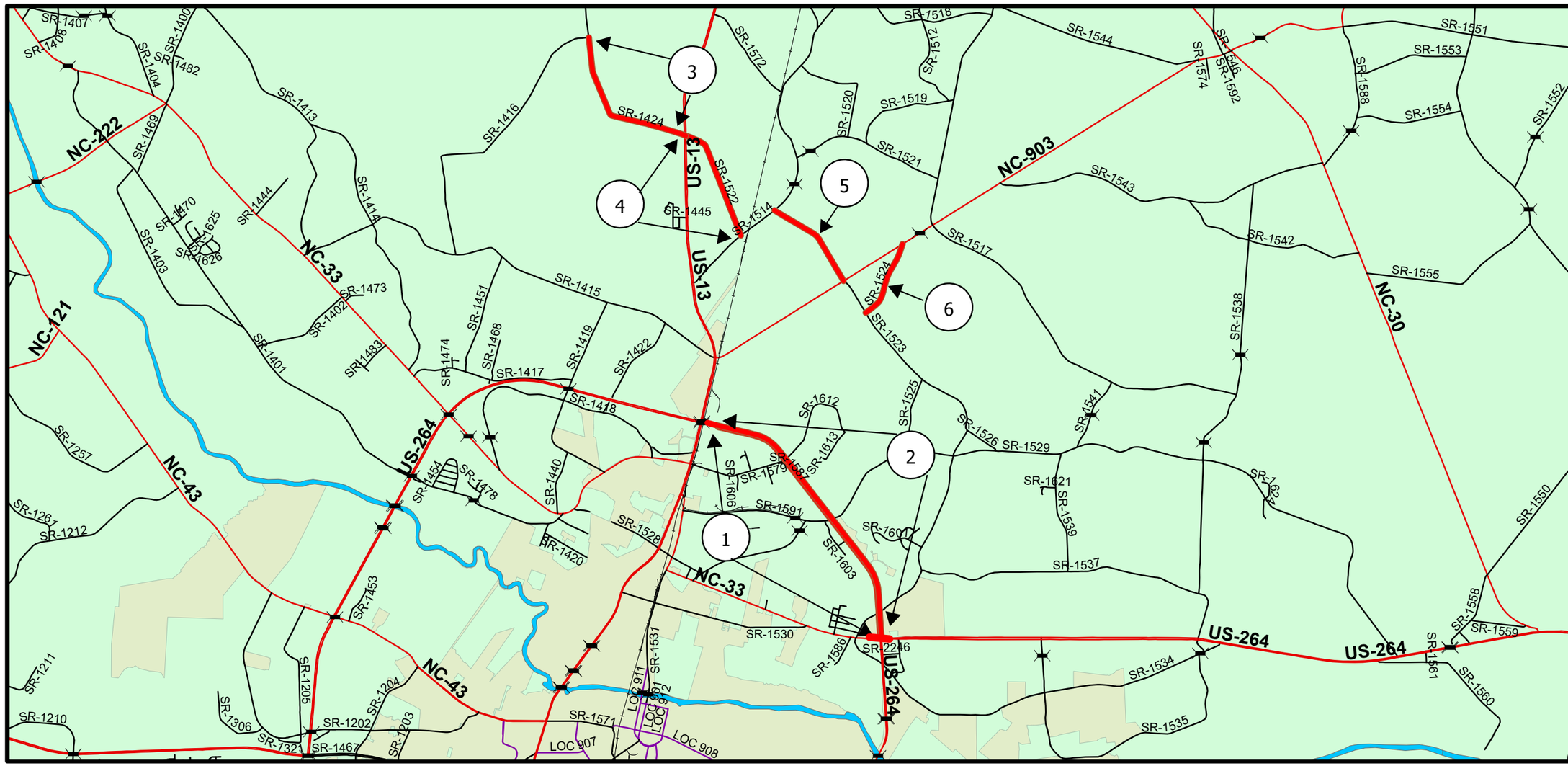
DB00573

WBS# 2024CPT.02.03.10741
2024CPT.02.26.20741

PROJECT REFERENCE NO.	SHEET NO.
DB00573	1

TYPE OF WORK : MILLING, STRENGTHENING, RESURFACING, AND SHOULDER RECONSTRUCTION

NCDOT
DIVISION 2



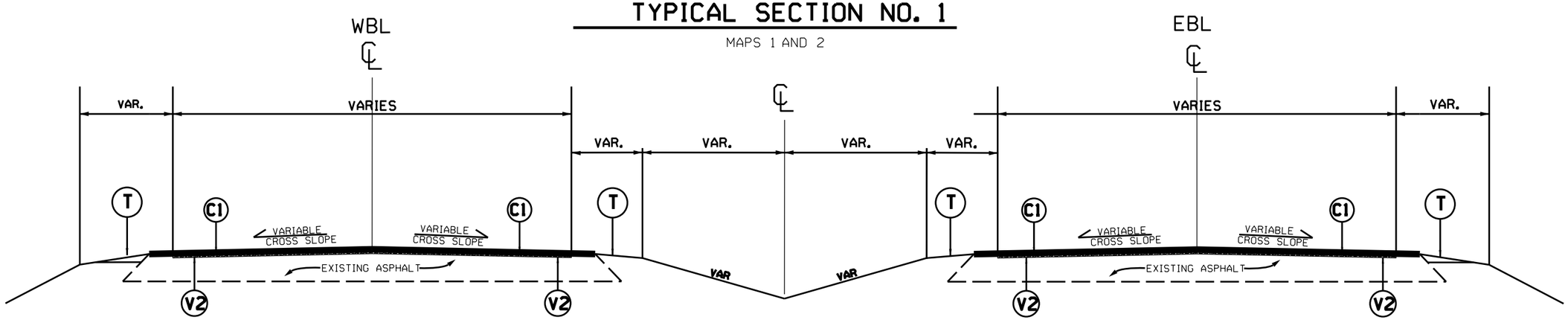
PROJECT REFERENCE NO.	SHEET NO.
DB00573	2

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	INCIDENTAL MILLING.
V2	MILLING DEPTH 1.5" FOR THE ENTIRE WIDTH OF ROADWAY.
DRAWINGS NOT TO SCALE	

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

TYPICAL SECTION NO. 1

MAPS 1 AND 2



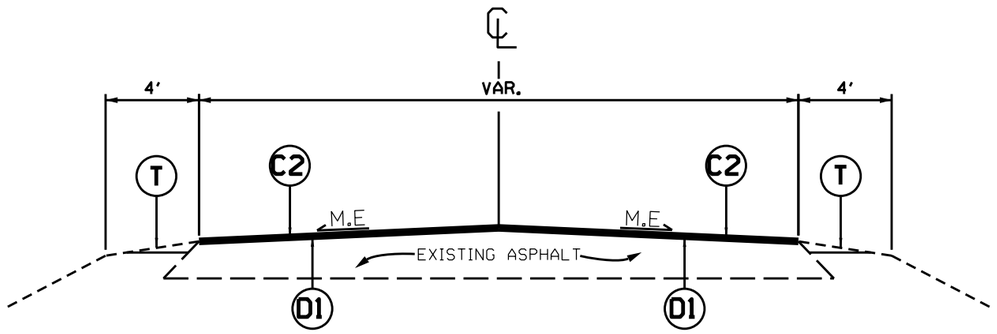
NOTE:

1. MILL FULL WIDTH OF THE ENTIRE ROADWAY TO A DEPTH OF 1.5 INCHES, MILLING TO INCLUDE BOTH NCDOT AND CITY SIDE STREETS TO THE BACK OF THE RADIUS.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF THE EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF THE MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.
4. TIE-IN TO THE BRIDGE APPROACH AT US 13 ON MAPS 1 AND 2.
5. MAP 1 ENDS JUST EAST OF NORTH DRIVE AND MAP 2 BEGINS JUST EAST OF BOSTIC DRIVE.
6. TIE-IN TO THE PAVEMENT JOINT ON NC 33 (PACTOLUS HIGHWAY) AND ON US 264 ALT JUST NORTH OF (SR-2246) MARINE DRIVE .
7. PERFORM SHOULDER RECONSTRUCTION AFTER PAVING IS COMPLETED.

PROJECT REFERENCE NO.	SHEET NO.
DB00573	3

TYPICAL SECTION NO. 2

MAP 3,4,5, AND 6



NOTE:

1. PLACE ASPHALT INTERMEDIATE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
2. PLACE ASPHALT SURFACE COURSE AT FULL WIDTH OF EXISTING ASPHALT PAVEMENT, AS DIRECTED BY THE ENGINEER.
3. INCLUDES INCIDENTAL MILLING AT THE ENDS OF MAIN LINE AND Y-LINE SECTIONS, AS DIRECTED BY THE ENGINEER.
4. PERFORM SHOULDER RECONSTRUCTION AFTER PAVING IS COMPLETED.

PAVEMENT SCHEDULE	
C1	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5C AT AN AVERAGE RATE OF 168 LBS. PER SQ. YD.
C2	PROP. APPROX. 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE S9.5B AT AN AVERAGE RATE OF 165 LBS. PER SQ. YD.
D1	PROP. APPROX. 2.5" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I19.0C AT AN AVERAGE RATE OF 285 LBS. PER SQ. YD.
T	SHOULDER RECONSTRUCTION
V1	INCIDENTAL MILLING.
V2	MILLING DEPTH 1.5" FOR THE ENTIRE WIDTH OF ROADWAY.
DRAWINGS NOT TO SCALE	

NOTE: PAVEMENT EDGE SLOPES ARE 1:1 UNLESS SHOWN OTHERWISE.

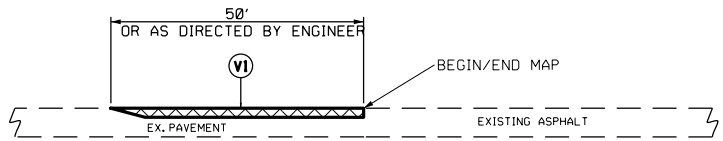
PROJECT NO.	SHEET NO.	TOTAL NO.
DB00573	4	

SUMMARY OF QUANTITIES

													0262000000-N	1220000000-E	1245000000-E	1297000000-E	1330000000-E	1503000000-E	1519000000-E	1523000000-E	1575000000-E	2830000000-N	6000000000-E	0710100000	6084000000-E	6117000000-N	4413000000-E	4457000000-N	4510000000-N
PROJECT NO	COUNTY	MAP NO	ROUTE	DESCRIPTION	TYP NO	LANES	LANE TYPE	FINAL SURFACE TESTING REQUIRED	WARM MIX ASPHALT REQUIRED	LENGTH	WIDTH	HAULING NCDOT SUPPLIED SHOULDER MATERIAL	INCIDENTAL STONE BASE	SHOULDER RECONSTRUCTION	1½" MILLING	INCIDENTAL MILLING	INTERMEDIATE COURSE, I19.0C	SURFACE COURSE, S9.5B	SURFACE COURSE, S9.5C	ASPHALT BINDER FOR PLANT MIX	ADJ. OF MANHOLES	TEMPORARY SILT FENCE	WATTLE	SEED & MULCHING	RESPONSE FOR EROSION CONTROL	WORK ZONE ADVANCE/ GENERAL WARNING SIGNING	TEMPORARY TRAFFIC CONTROL	LAW ENFORCEMENT
										MI	FT	EA	TONS	SMI	SY	SY	TONS	TONS	TONS	TONS	EA	LF	LF	AC	EA	SF	LS	HR
2024CPT.02.03.10741	Pitt	1	US-264 E	FROM US 13 (BRIDGE) TO US 264 (EAST OF NORTH DR.)	1	2	2WD	NO	NO	3.32	30	133	33	6.64	68,579	8,000			6,700	395		531	664	3.32	1	400	0.30	40
TOTAL FOR MAP NO. 1										**3.32**		**133**	**33**	**6.64**	**68,579**	**8,000**			**6,700**	**395**		**531**	**664**	**3.32**	**1**	**400**	**0.30**	**40**
2024CPT.02.03.10741	Pitt	2	US-264 W	FROM US 264 (EAST OF BOSTIC DR.) TO US 13 (BRIDGE)	1	2	2WD	NO	NO	3.35	30	134	34	6.70	74,500	2,600			6,800	401		536	100	3.35		400	0.30	40
TOTAL FOR MAP NO. 2										**3.35**		**134**	**34**	**6.70**	**74,500**	**2,600**			**6,800**	**401**		**536**	**100**	**3.35**		**400**	**0.30**	**40**
TOTAL FOR PROJ NO. 2024CPT.02.03.10741										**6.67**		**267**	**67**	**13.34**	**143,079**	**10,600**			**13,500**	**796**		**1,067**	**764**	**6.67**	**1**	**800**	**0.60**	**80**
2024CPT.02.26.20741	Pitt	3	SR-1424 / ALLPINE-TAYLOR RD	FROM US 13 TO END OF PAVEMENT	2	2	2WU	NO	NO	1.7	21	68	68	3.40		500	3,126	1,834		269	4	272	100	1.70		250	0.16	
TOTAL FOR MAP NO. 3										**1.7**		**68**	**68**	**3.40**		**500**	**3,126**	**1,834**		**269**	**4**	**272**	**100**	**1.70**		**250**	**0.16**	
2024CPT.02.26.20741	Pitt	4	SR-1522 / FUTRELL-ROBSON RD	FROM SR 1514 STATON MILL RD. TO US 13	2	2	2WU	NO	NO	1.24	20	50	50	2.48		250	2,150	1,246		184		198		1.24		140	0.10	
TOTAL FOR MAP NO. 4										**1.24**		**50**	**50**	**2.48**		**250**	**2,150**	**1,246**		**184**		**198**		**1.24**		**140**	**0.10**	
2024CPT.02.26.20741	Pitt	5	SR-1523 / WHICHARD RD	FROM NC 903 T SR 1514 STATON MILL ROAD	2	2	2WU	NO	NO	1.07	25	54	43	2.14		250	2,340	1,362		201		171	50	1.07	1	150	0.08	
TOTAL FOR MAP NO. 5										**1.07**		**54**	**43**	**2.14**		**250**	**2,340**	**1,362**		**201**		**171**	**50**	**1.07**	**1**	**150**	**0.08**	
2024CPT.02.26.20741	Pitt	6	SR-1524 / BROWN RD	FROM SR 1523 WHICHARD RD. TO NC 903	2	2	2WU	NO	NO	0.79	21	40	32	1.58		250	1,410	840		122		126	50	0.79		125	0.06	
TOTAL FOR MAP NO. 6										**0.79**		**40**	**32**	**1.58**		**250**	**1,410**	**840**		**122**		**126**	**50**	**0.79**		**125**	**0.06**	
TOTAL FOR PROJ NO. 2024CPT.02.26.20741										**4.8**		**212**	**193**	**9.60**		**1,250**	**9,026**	**5,282**		**776**	**4**	**767**	**200**	**4.80**	**1**	**665**	**0.40**	
GRAND TOTAL										**11.47**		**479**	**260**	**22.94**	**143,079**	**11,850**	**9,026**	**5,282**	**13,500**	**1,572**	**4**	**1,834**	**964**	**11.47**	**2**	**1,465**	**1**	**80**

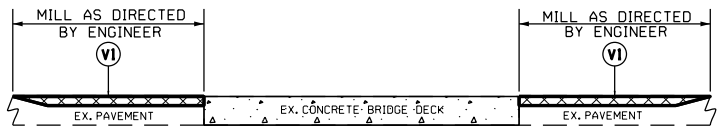
PROJECT REFERENCE NO.	SHEET NO.
DB00573	DIV2-1

MILLING TYPICALS



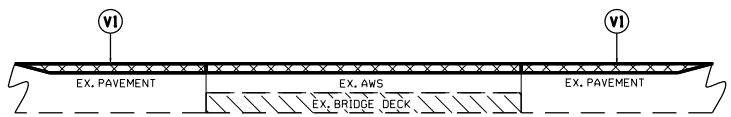
DETAIL 1
BEGIN/END MAP TIE-IN

NOTE:
1. MILLING SHALL BE PERFORMED AT MAIN LINE TIE-INS AND Y-LINE TIE-INS AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 2
BRIDGE MILLING

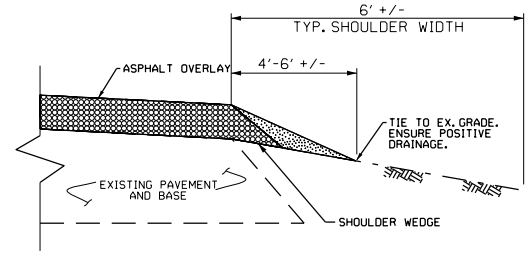
NOTE:
1. MILLING SHALL BE PERFORMED AT THE BRIDGE APPROACHES AS DIRECTED BY THE ENGINEER, IN ACCORDANCE WITH THIS DETAIL.



DETAIL 3
BRIDGE MILLING

NOTE:
1. INCLUDES MILLING FOR THE ENTIRE WIDTH OF THE BRIDGE WEARING SURFACE, AS DIRECTED BY THE ENGINEER.

SHOULDER RECONSTRUCTION TYPICAL

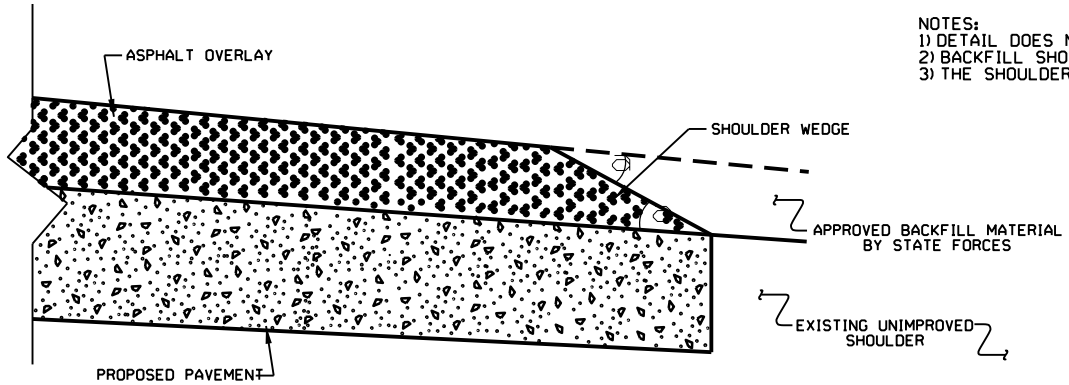


SHOULDER RECONSTRUCTION DETAIL

NOTE:
1. SHOULDERS SHALL BE RECONSTRUCTED AS SHOWN IN STD. DWG. NO. 560.01 & 560.02, WITH A MINIMUM SLOPE OF 1" PER FOOT TO ENSURE POSITIVE DRAINAGE AWAY FROM THE ROADWAY.
2. A VEGETATIVE BUFFER SHALL BE MAINTAINED BETWEEN THE DISTURBED AREA ALONG THE EDGE OF PAVEMENT AND THE DITCH SHOULDER POINT TO MINIMIZE EROSION. PULLING DITCHES OR CUTTING SHOULDERS TO GENERATE BORROW MATERIAL WILL NOT BE ALLOWED.
3. REQUIRED BORROW MATERIAL MAY BE OBTAINED FROM NCDOT STOCKPILES. ANY EXCESS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR IN AN APPROVED DISPOSAL SITE.

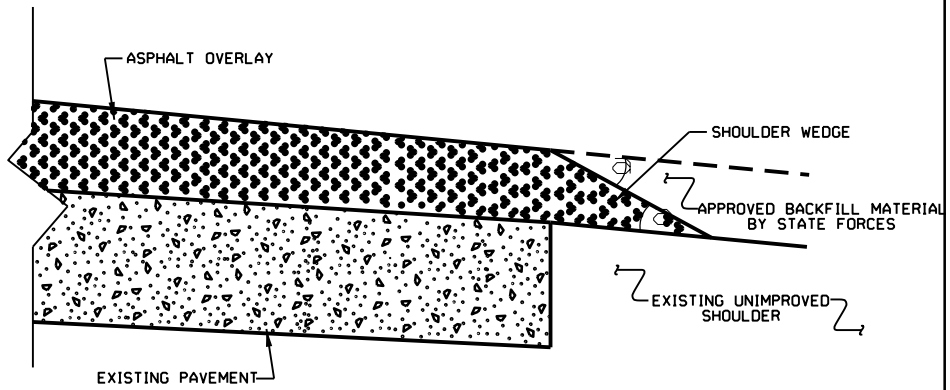
PROJECT REFERENCE NO.	SHEET NO.
DB00573	DIV2-2

NOTES:
1) DETAIL DOES NOT APPLY TO OGAFC AND ULTRA-THIN BONDED WEARING COURSE.
2) BACKFILL SHOULDER WITH APPROVED MATERIAL.
3) THE SHOULDER WEDGE DEVICE MAY BE DISENGAGED AT PAVED DRIVEWAYS AND SIDE STREETS.



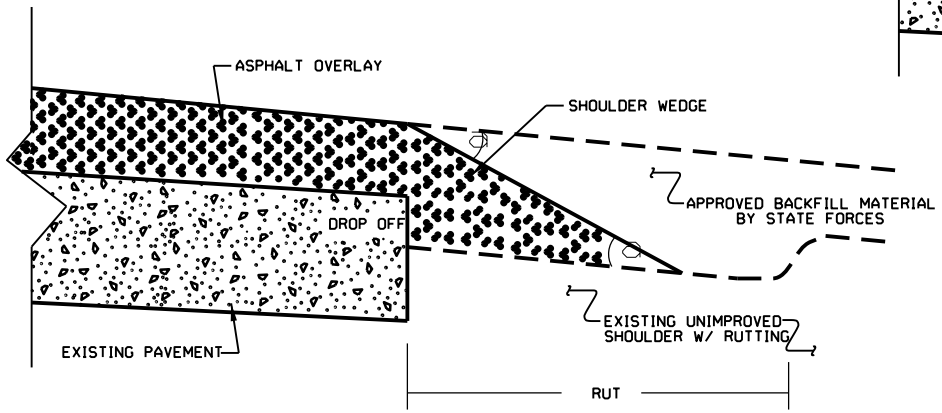
SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ Widening or with Existing Paved Shoulder having no dropoffs)



SHOULDER WEDGE DETAIL

(Resurfacing Projects w/ NO Widening)



SHOULDER WEDGE DETAIL

(Resurfacing Adjacent to Rutted Shoulder)

- SHOULDER WEDGE ANGLE = 30°

CONTRACT STANDARDS AND DEVELOPMENT UNIT
Office 919-707-6950 Fax 919-250-4119

SHOULDER WEDGE DETAILS

ORIGINAL BY: T.SPELL DATE: 7-19-11
MODIFIED BY: DATE: 10/16/12
CHECKED BY: DATE:
FILE SPEC'd

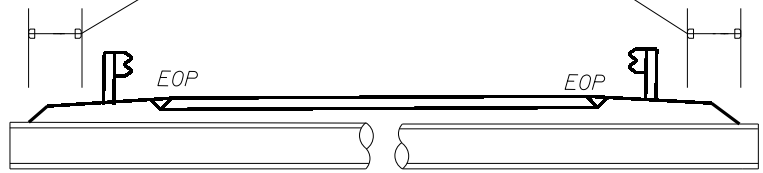
PROJECT REFERENCE NO.	SHEET NO.
DB00573	EC-1

NOTES: Less than 5' - 10' undisturbed buffer from ROW, ditchline, water feature, or drainage inlet, add BMP.

BMP Options: Wattle, Silt Fence or Hardened Aggregate.

EROSION CONTROL DETAIL

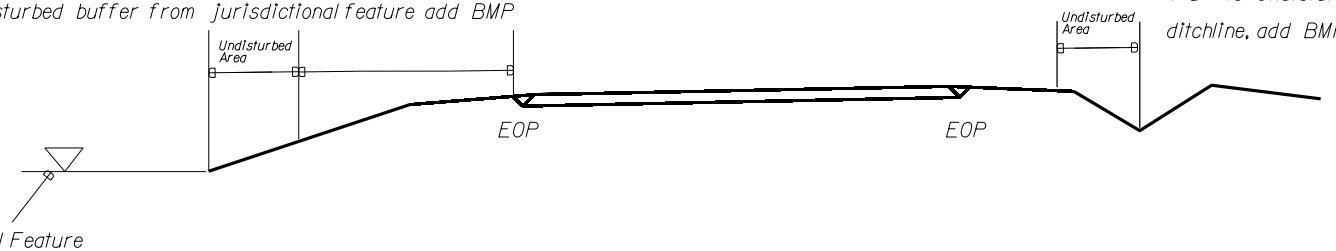
< 5' - 10' Undisturbed buffer add BMP



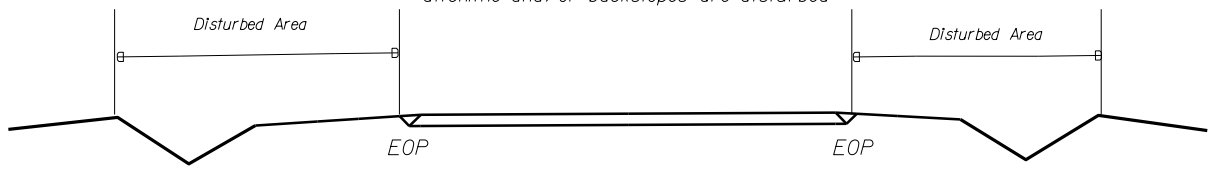
Pipe/Culvert

< 5' - 10' Undisturbed buffer from jurisdictional feature add BMP

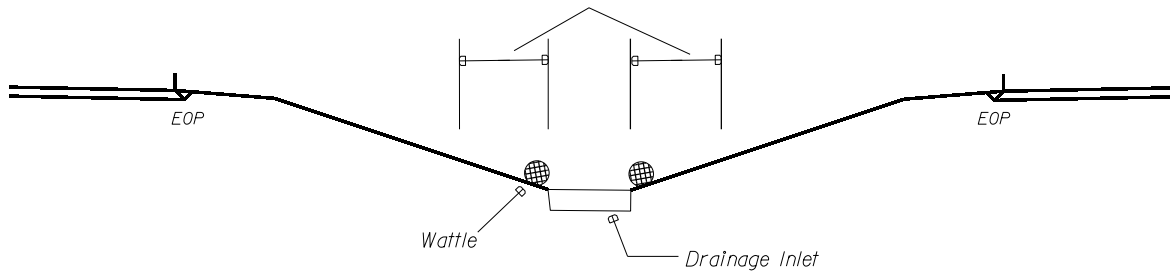
< 5' - 10' Undisturbed buffer from ditchline, add BMP



Use BMP's if shoulders and/or frontslopes and/or ditchline and/or backslopes are disturbed



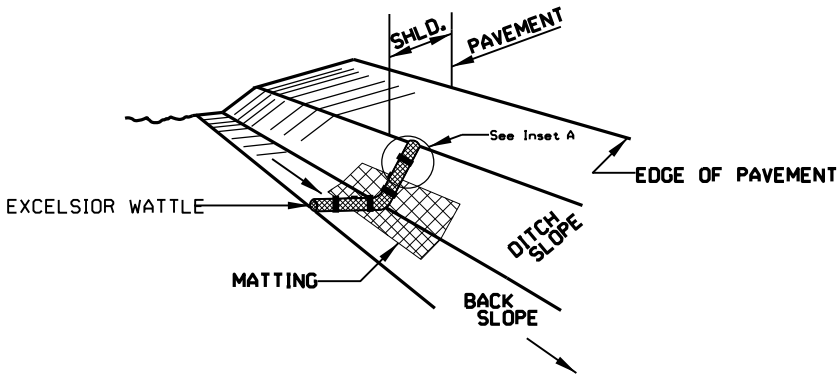
< 5' - 10' Undisturbed buffer from inlet, add wattle



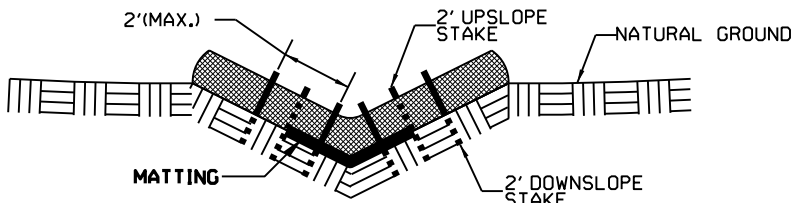
NOT TO SCALE

PROJECT REFERENCE NO.	SHEET NO.
DB00573	EC-2

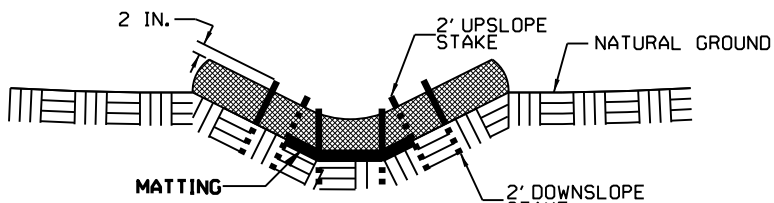
WATTLE DETAIL



ISOMETRIC VIEW



CROSS SECTION
VEE DITCH



CROSS SECTION
TRAPEZOIDAL DITCH

NOTES:

USE MINIMUM 12 IN. DIAMETER EXCELSIOR WATTLE.

USE 2 FT. WOODEN STAKES WITH A 2 IN. BY 2 IN. NOMINAL CROSS SECTION.

ONLY INSTALL WATTLE(S) TO A HEIGHT IN DITCH SO FLOW WILL NOT WASH AROUND WATTLE AND SCOUR DITCH SLOPES AND AS DIRECTED.

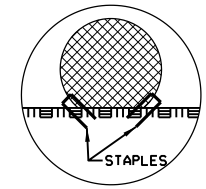
INSTALL A MINIMUM OF 2 UPSLOPE STAKES AND 4 DOWNSLOPE STAKES AT AN ANGLE TO WEDGE WATTLE TO BOTTOM OF DITCH.

PROVIDE STAPLES MADE OF 0.125 IN. DIAMETER STEEL WIRE FORMED INTO A U SHAPE NOT LESS THAN 12" IN LENGTH.

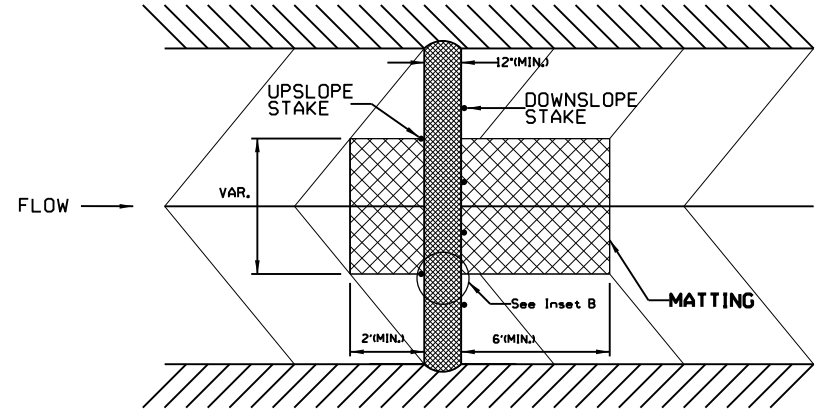
INSTALL STAPLES APPROXIMATELY EVERY 1 LINEAR FOOT ON BOTH SIDES OF WATTLE AND AT EACH END TO SECURE IT TO THE SOIL.

INSTALL MATTING IN ACCORDANCE WITH SECTION 1631 OF THE STANDARD SPECIFICATIONS.

INSET A



INSET B

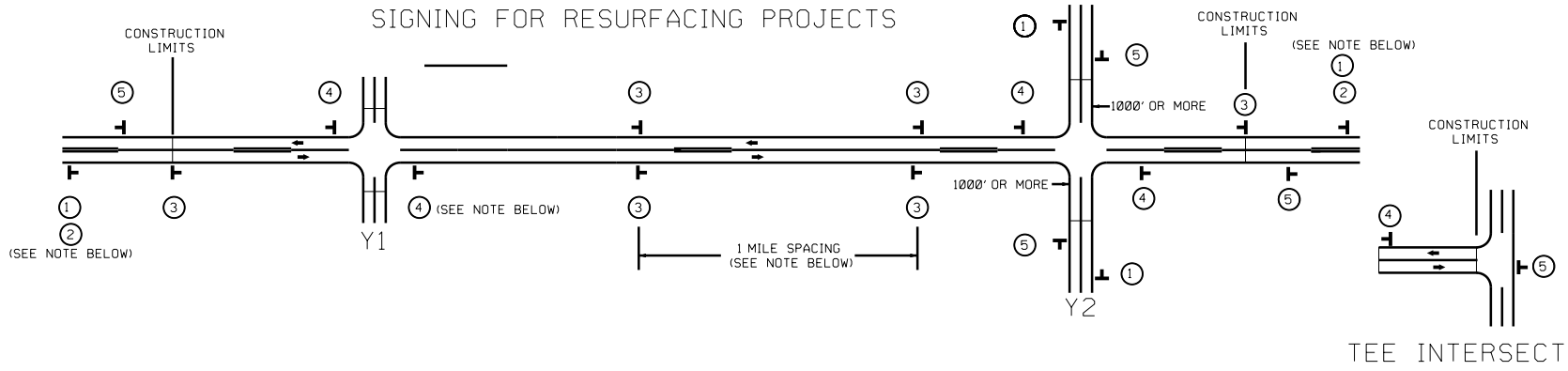


TOP VIEW

NOT TO SCALE

PROJECT REFERENCE NO.	SHEET NO.
DB00573	TCP-1

SIGNING FOR RESURFACING PROJECTS



LEGEND
- STATIONARY SIGN
- DIRECTION OF TRAFFIC FLOW

MAINLINE (-L-) SIGNING

SIGNING NOTES AND PLACEMENT PER DIRECTION

1 2	ROAD WORK AHEAD W20-1 48" X 48" NEXT XX MILES W7-3aP 24" X 18"	PLACE 1000' PRIOR TO BEGINNING OF CONSTRUCTION LIMITS. ONLY USED ON -Y- LINES IF RESURFACING LIMITS EXTEND 1000' ALONG -Y- LINE "2 SIGN ONLY USED WHEN RESURFACING LIMITS ARE 2 OR MORE MILES IN LENGTH. ROUND UP TO NEXT WHOLE NUMBER.(NO FRACTIONAL OR DECIMAL NUMBERS)
3	LOW/SOFT SHOULDER SP 13107 48" X 48"	- PLACE INITIALLY AT THE CONSTRUCTION LIMITS AND SPACE 1 MILE APART THEREAFTER. - AT TEE INTERSECTIONS INSTALL INITIALLY ½ MILE FROM INTERSECTION AND SPACE 1 MILE APART THEREAFTER.
4	ROAD UNDER CONST SP 13106 48" X 48"	- THESE ARE FOR -Y- LINES THAT ARE "THROUGH" ROADWAYS. - DEAD END AND SUBDIVISION ROADS ARE NOT "THROUGH" ROADWAYS. - INSTALL 500' +/- FROM EACH -Y- LINE APPROACH AS SHOWN ABOVE. - FOR MULTIPLE -Y- LINES THAT ARE SEPARATED BY 0.25 MILES OR LESS, TREAT AS A SINGLE UNIT AND INSTALL WITHIN 500' OF EACH APPROACH. - A MAXIMUM OF 2 SIGN SETS PER MILE. DO NOT INSTALL WHEN -Y- LINES ARE WITHIN 0.5 MILES FROM "END ROAD WORK" SIGN. - FOR TEE INTERSECTIONS, INSTALL WITHIN 500' +/- OF THE INTERSECTION ALONG -L- LINE.
5	END ROAD WORK C20-2 A 48" X 24"	PLACE 500' FOLLOWING THE END OF CONSTRUCTION LIMITS OR AS SHOWN WHEN WORK ENDS AT A 3-WAY TEE INTERSECTION.

-Y- LINE SIGNING

NO REQUIRED STATIONARY SIGNING FOR THE FOLLOWING -Y- LINE CONDITIONS:

1) LESS THAN 1000' OF RESURFACING ALONG -Y- LINE
2) SUBDIVISION ROADS
3) DEAD END ROADS

WHEN PAVING/CONSTRUCTION ACTIVITIES PROCEED ACROSS AN UNSIGNED -Y- LINE, ADVANCE WARNING PORTABLE SIGNS SHALL BE USED ALONG THE -Y- LINE AS SHOWN BELOW. REMOVE UPON COMPLETION OF WORK.

ROAD WORK AHEAD W20-1 48" X 48"

W20-7 A 48" X 48"

PLACED 500' IN ADVANCE OF FLAGGER. PLACED 250' IN ADVANCE OF FLAGGER.

DIVISION OF HIGHWAYS • STATE OF NORTH CAROLINA • DEPARTMENT OF TRANSPORTATION • WORK ZONE TRAFFIC CONTROL

RESURFACING ADVANCE WARNING SIGNS FOR RURAL AND SUBURBAN 2 LANE ROADWAYS